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 TENNESSEE DEPARTMENT OF TRANSPORTATION
 ADMINISTRATION BUILDING, FIRST FLOOR
 300 BENCHMARK PLACE
 JACKSON, TN 38301
 ELIZABETH DAVID, P.E. NO. 124129

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND STANDARD TRAFFIC DESIGN DRAWINGS	1A
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B – 2B2
GENERAL NOTES	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES	2E – 2E1
TABULATED QUANTITIES	2F – 2F1
UTILITIES NOTES AND UTILITY OWNERS	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1

YEAR	PROJECT NO.	SHEET NO.
2026	NH/HSIP-43(50)	ROADWAY-SIGN1

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**SIGNATURE
SHEET**

26-MAR-2026 13:19
\\TDO04NAS002.tdot.state.tn.us\04shared\Design\DESIGN\RESURF REG4 PROJ\GIBSON\SR43\LM14-25\LM24-26 (132954-00)\SHEETS\SHEET 0 - SIGNATURE2.dgn



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SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN2
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND STANDARD TRAFFIC DESIGN DRAWINGS	1A
ESTIMATED ROADWAY QUANTITIES	2

YEAR	PROJECT NO.	SHEET NO.
2026	NH/HSIP-43(50)	ROADWAY-SIGN2

REV. 03/27/26: ADDED SHEET

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET



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TENNESSEE DEPARTMENT OF TRANSPORTATION
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SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN3
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND STANDARD TRAFFIC DESIGN DRAWINGS	1A
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B - 2B1
GENERAL NOTES.....	2C
SPECIAL NOTES.....	2D
ENVIRONMENTAL NOTES.....	2E
TABULATED QUANTITIES	2F1
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	T1

YEAR	PROJECT NO.	SHEET NO.
2026	NH/HSIP-43(50)	ROADWAY-SIGN3

REV. 06/02/26: ADDED SHEET

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

SIGNATURE
 SHEET

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION BUREAU OF ENGINEERING

GIBSON COUNTY

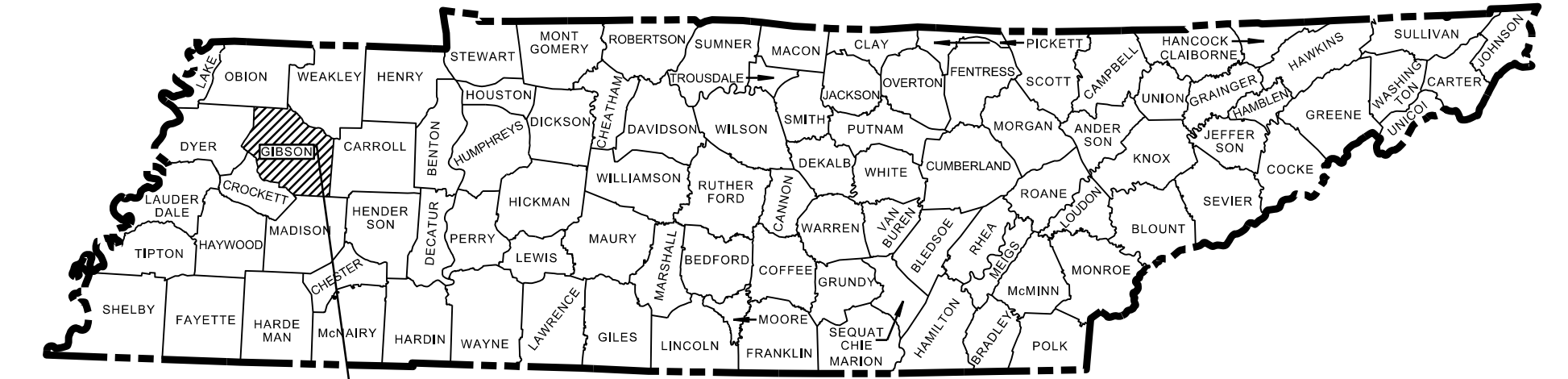
STATE ROUTE 43
FROM NEAR CADES ATWOOD ROAD
TO WEAKLEY COUNTY LINE

RESURFACE & SAFETY
411D MIX, COLD PLANING, PAVEMENT MARKINGS,
SNOWPLOWABLE MARKERS & GUARDRAIL TERMINALS
STATE HIGHWAY NO. 43 F.A.H.S. NO. 45E

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X
RAILROAD INVOLVEMENT	YES X	NO

TENN.	YEAR	SHEET NO.
	2026	1
FED. AID PROJ. NO.	NH/HSIP-43(50)	
STATE PROJ. NO.	27S043-F3-002 27S043-F8-002	

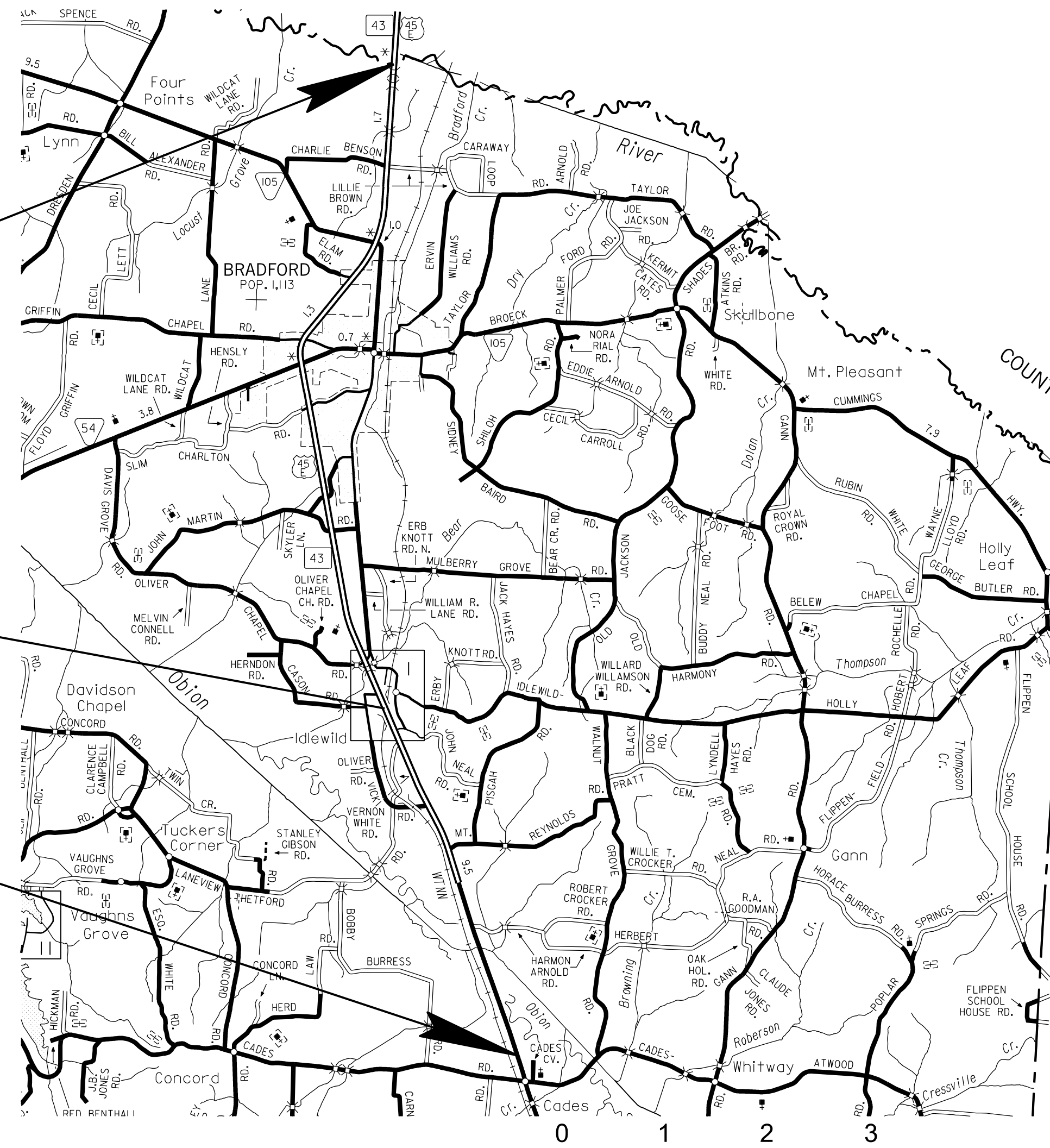
REV. 06/02/26 : REVISED TREATMENT TYPE



PROJECT LOCATION

BRIDGE ID. #27SR0430011, 27SR0430012, 27SR0430013,
27SR0430014, 27SR0430031, 27SR0430032,
27SR0430033, 27SR0430019, 27SR0430021,
27SR0430023

NO EXCLUSIONS



27S043-F3-002
27S043-F8-002
END PROJECT NO. NH/HSIP-43(50) RESURFACE & SAFETY
WEAKLEY COUNTY LINE (L.M. 24.26)

WEST TENNESSEE RAILROAD, LLC (WTNN)
#299167P @ S.R. 43 L.M. 17.69
RR MP 0440.300
LAT: 36.0263080
LONG: -88.8092183

27S043-F3-002
27S043-F8-002
BEGIN PROJECT NO. NH/HSIP-43(50) RESURFACE & SAFETY
NEAR CADES ATWOOD ROAD (L.M. 14.18)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED APRIL 1, 2026 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER : ELI JONES, REG. 4

DESIGNER : RYAN LINDSAY, REG. 4

CHECKED BY : ELIZABETH DAVID, P.E., REG. 4

P.E. NO. 98043-4283-04 (DESIGN)

PIN NO. 132954.00

PROJECT LENGTH 10.08 MILES
TOTAL LANE MILES RESURFACED 40.80 MILES

TRAFFIC COUNTER & WEATHER STATIONS

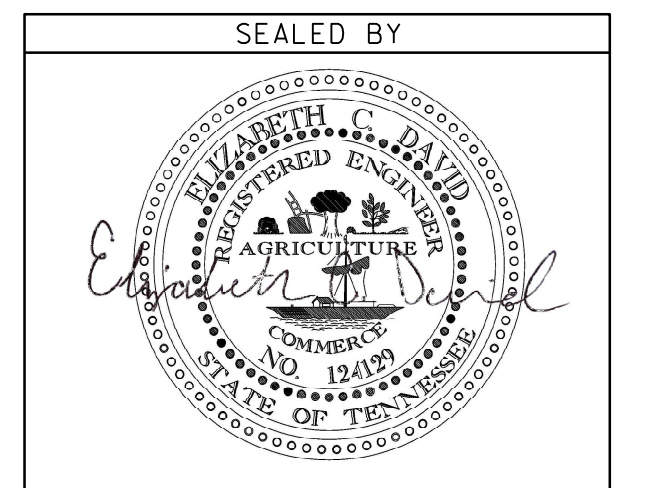
STATION LOCATION	LOG MILE
TC STATION 173	16.01
TC STATION 232	20.06
TC STATION 233	21.86

POSTED SPEED LIMITS

L.M. 14.18 - L.M. 14.25	50 MPH
L.M. 14.25 - L.M. 20.33	65 MPH
L.M. 20.33 - L.M. 21.55	55 MPH
L.M. 21.55 - L.M. 24.26	65 MPH

TRAFFIC DATA

ADT (2026)	7,004
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APPROVED:

Shane Hester

SHANE HESTER, CHIEF ENGINEER

DATE:

APPROVED:

Will Reid

WILL REID, COMMISSIONER

02-JUN-2026 09:09 \\TDO\T04NAS002.tdot.state.tn.us\04Shared\Design\DESIGN\RESURF REG4 PROJ\GIBSON\SR43\LM14-25\LM24-26 (132954-00)\SHEETS\SHEET 1A - ROADWAY INDEX.dgn

ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
SIGNATURE SHEET	ROADWAY-SIGN2
SIGNATURE SHEET	ROADWAY-SIGN3
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NOTES:

NO UTILITY SHEETS INCLUDED IN THIS PROJECT.
 NO PROJECT COMMITMENTS INCLUDED IN THIS SET OF PLANS.

STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
10-100.00 STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	01-30-26	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND

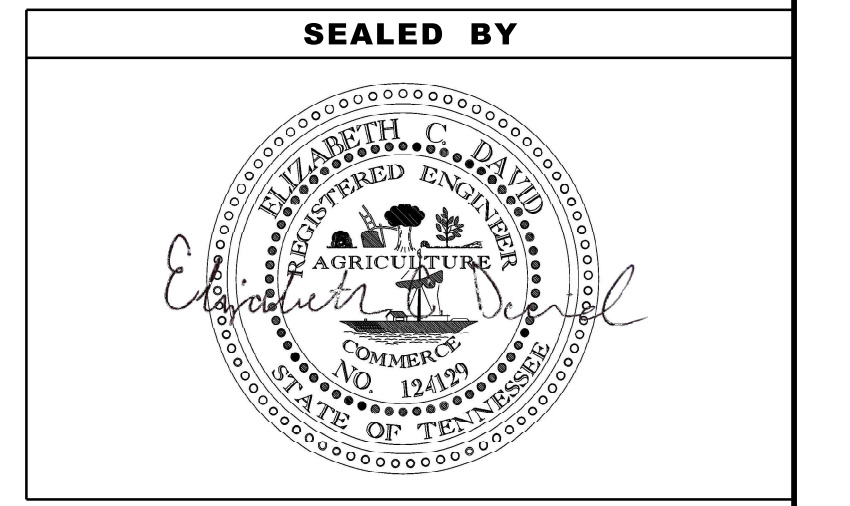
DWG.	REV.	DESCRIPTION
10-108.00 SAFETY DESIGN AND GUARDRAILS		
S-GRS-4	05-04-22	SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL

STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
10-200.00 SIGN		
T-S-19	07-30-25	STANDARD STEEL SIGN SUPPORTS
T-S-20	07-30-25	SIGN DETAILS
T-S-23A	07-30-25	MULTI-DIRECTIONAL SLIP BASE BREAKAWAY P-POST SIGN SUPPORT
T-S-26	07-30-25	SIGNING PLAN FOR RESTRICTED CROSSING & J-TURN INTERSECTIONS
10-204.00 DESIGN – TRAFFIC CONTROL		
T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	01-24-25	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-19	01-24-25	PAVEMENT MARKING DETAILS FOR RESTRICTED CROSSING & J-TURN INTERSECTION
T-WZ-10	03-26-25	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-26-25	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-40	03-26-25	RIGHT LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-41	03-26-25	LEFT LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-FAB1	03-26-25	FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	1A

REV. 03/27/26: ADDED ROADWAY-SIGN2 TO ROADWAY INDEX
 REV. 06/02/26: ADDED ROADWAY-SIGN3. REVISED QUANTITY OF TYPICAL SHEETS. ADDED SECTION NUMBERS TO STANDARD SUBHEADINGS. REORDERED THE PVMT. MARKINGS AND WORK ZONES SECTIONS



**STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION**

ROADWAY INDEX, STANDARD
 ROADWAY DRAWINGS AND
 STANDARD TRAFFIC
 DESIGN DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	2

ESTIMATED ROADWAY QUANTITIES

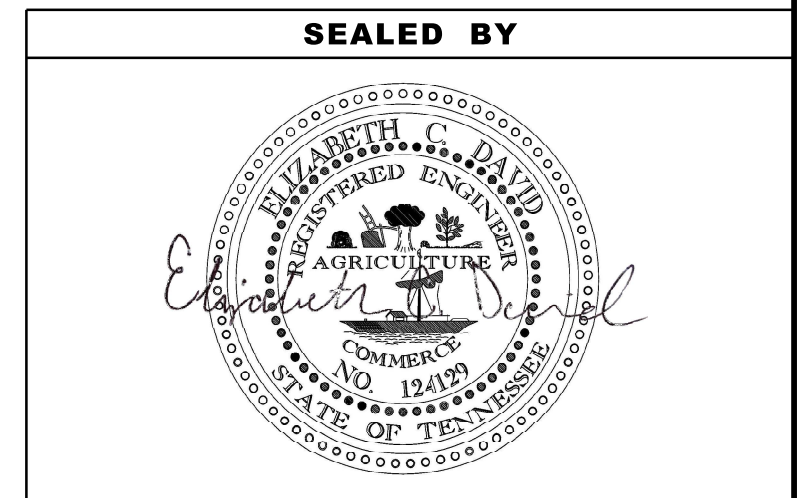
ITEM NO.	DESCRIPTION	UNIT	QUANTITY		TOTAL QUANTITY
			27S043-F8-002	27S043-F3-002	
(1)	202-03.01 REMOVAL OF ASPHALT PAVEMENT	S.Y.	707		707
	203-06 WATER	M.G.	1		1
(2)	208-01.05 BROOMING & DEGRASSING SHOULDERS	L.M.	37.8		37.8
(3)	303-02 MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)	TON	99		99
(3)	307-01.01 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	66		66
(4)	307-01.08 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	127		127
(5)	403-01.10 HIGH PERFORMANCE FOG SEALS	S.Y.	113046		113046
(6)	403-02.01 TRACKLESS TACK COAT	TON	150		150
(7)	411-01.10 ACS MIX(PG64-22) GRADING D	TON	22868		22868
(5)(8)(9)(10)	411-04 CRACK SEALANT	LB.	8564		8564
(11)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	12254		12254
(12)	705-02.10 GUARDRAIL TRANSITION 27IN TO 31IN	EACH		31	31
(12)	705-06.20 TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH		31	31
(12)	706-01 GUARDRAIL REMOVED	L.F.		1550	1550
	712-01 TRAFFIC CONTROL	LS	1		1
(13)	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	60		60
(14)(15)	712-06 SIGNS (CONSTRUCTION)	S.F.	6172		6172
	712-08.03 ARROW BOARD (TYPE C)	EACH	4		4
(16)	713-15.01 REMOVAL OF EXISTING SIGN POST	EACH		4	4
(16)(17)	713-16.20 SIGNS (STOP R1-1)	EACH		2	2
(16)(17)	713-16.21 SIGNS (DO NOT ENTER R5-1)	EACH		2	2
(16)(17)	713-16.22 SIGNS (DO NOT ENTER R5-1, ONE WAY R6-1L)	EACH		2	2
(16)(17)	713-16.23 SIGNS (YIELD R1-2, NO RIGHT TURN R3-1)	EACH		2	2
	716-01.21 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH	222		222
	716-01.22 SNOWPLOWABLE RAISED PAVEMENT MARKERS (MONO-DIR)(1 COLOR)	EACH	195		195
	716-01.23 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	1223		1223
(18)	716-01.30 REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	1490		1490
(19)(20)	716-02.04 PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	117		117
(19)(20)(21)	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	550	50	600
(19)(20)	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	24		24
(19)(20)(22)	716-02.08 PLASTIC PAVEMENT MARKING (8" DOTTED LINE)	L.F.		1497	1497
(19)(20)(23)(24)	716-02.11 PLASTIC PAVEMENT MARKING (6" DOTTED LINE)	L.F.	95	466	561
(19)(20)	716-02.12 PLASTIC PAVEMENT MARKING (8IN LINE)	L.M.	0.5		0.5
(19)(20)(24)	716-02.15 PLASTIC PAVEMENT MARKING (U TURN ARROW)	EACH	4		4
(25)	716-05.20 PAINTED PAVEMENT MARKING (6" LINE)	L.M.	44		44
(19)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	44		44
	717-01 MOBILIZATION	LS	1		1

FOOTNOTES

- (1) INCLUDES 147 S.Y. FOR FULL-DEPTH ASPHALT REPAIR AND 560 S.Y. FOR PARTIAL-DEPTH ASPHALT REPAIR.
- (2) INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK. SEE SHEET 2C, FINAL PAVEMENT MARKING NOTE 6 FOR MORE INFORMATION.
- (3) ITEM TO BE USED FOR FULL-DEPTH ASPHALT REPAIR AREAS ONLY.
- (4) ITEM TO BE USED FOR PARTIAL-DEPTH ASPHALT REPAIR AREAS ONLY.
- (5) ITEM TO BE USED FOR TREATMENT OF OUTSIDE SHOULDERS ONLY.
- (6) INCLUDES 12 TONS FOR USE AT BULBOOTS, MEDIAN CROSSEOVERS, SIDE ROADS AND ASPHALT REPAIR.
- (7) INCLUDES 1840 TONS FOR USE AT BULBOOTS, MEDIAN CROSSEOVERS AND SIDE ROADS.
- (8) THE CONTRACTOR SHALL BE REQUIRED TO CLEAN THE AREA TO RECEIVE TREATMENT PRIOR TO THE APPLICATION OF THE PRODUCT. THIS SHALL BE ACCOMPLISHED BY A METHOD APPROVED BY THE TDOT PROJECT ENGINEER. THE COST OF CLEANING THE TREATMENT AREA SHALL NOT BE PAID FOR DIRECTLY BUT WILL BE INCLUDED IN THE COST OF OTHER ITEMS BID.
- (9) QUANTITIES SHOWN ARE FOR BID PURPOSES ONLY AND ACTUAL FIELD QUANTITIES SHALL BE USED FOR BASIS OF PAYMENT.
- (10) DO NOT APPLY TO FATIGUE CRACKING OF ANY SEGMENT.
- (11) INCLUDES 1839 TONS FOR ADDITIONAL MILLING AT BULBOOTS, MEDIAN CROSSEOVERS & SIDE ROADS.
- (12) SEE PROPOSED GUARDRAIL (RESURFACING) BLOCK ON SHEET 2F FOR DETAILS.
- (13) TO BE USED FOR TRAFFIC CONTROL TRANSITION TAPERS ONLY.
- (14) SEE TRAFFIC CONTROL SIGN TABULATION (RESURFACING) BLOCK ON SHEET 2F1 FOR DETAILS.
- (15) ALL SIGNING SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- (16) SEE PROPOSED SIGNS AND REMOVAL OF SIGNS BLOCK ON SHEET 2F FOR DETAILS.
- (17) SIGN FACE, SUPPORT, INSTALLATION AND HARDWARE ARE TO BE INCLUDED IN THIS ITEM.
- (18) TO BE USED AS DIRECTED BY THE TDOT ENGINEER.
- (19) FOR FINAL PAVEMENT MARKING ONLY.
- (20) THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- (21) ALL STOP BARS ON SIDE STREETS WILL BE RETRACED AS DIRECTED BY THE ENGINEER.
- (22) INCLUDES 125 L.F. FOR USE AT THE J-TURN LOCATION. SEE STD. DRAWING T-M-19 FOR DETAILS.
- (23) TO BE USED AS EDGE LINE MARKINGS AT MEDIAN CROSSEOVER LOCATIONS.
- (24) TO BE USED AT THE J-TURN LOCATION. SEE STD. DRAWING T-M-19 FOR DETAILS.
- (25) FOR TEMPORARY PAVEMENT MARKING ONLY.

REV. 03/27/26 : REMOVED ITEMS 415-01.01 & 712-06 FROM GENERAL. UPDATED ITEMS 415-01.01 & 712-06 WITHIN ALT AA1. ADDED ITEM 712-06 AND UPDATED ITEM 415-01.01 IN ALT AA2. REMOVED CRACK SEAL FOOTNOTE AND RENUMBERED

REV. 06/02/26: REMOVED ALTERNATIVE SECTIONS AA1 AND AA2. REMOVED ITEMS 414-04.03, 414-04.04, 716-08.04, 716-08.06, 716-08.19 AND 716-08.20. ADDED ITEMS 202-03.01, 203-06, 303-02, 307-01.01, 307-01.08 AND 411-01.10. REVISED ITEMS 403-01.10 AND 411-04. UPDATED AND REORDERED FOOTNOTES.

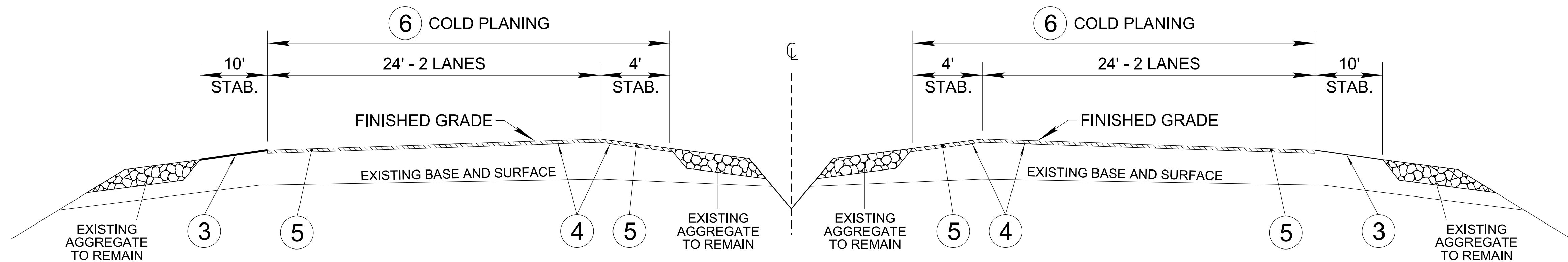


**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

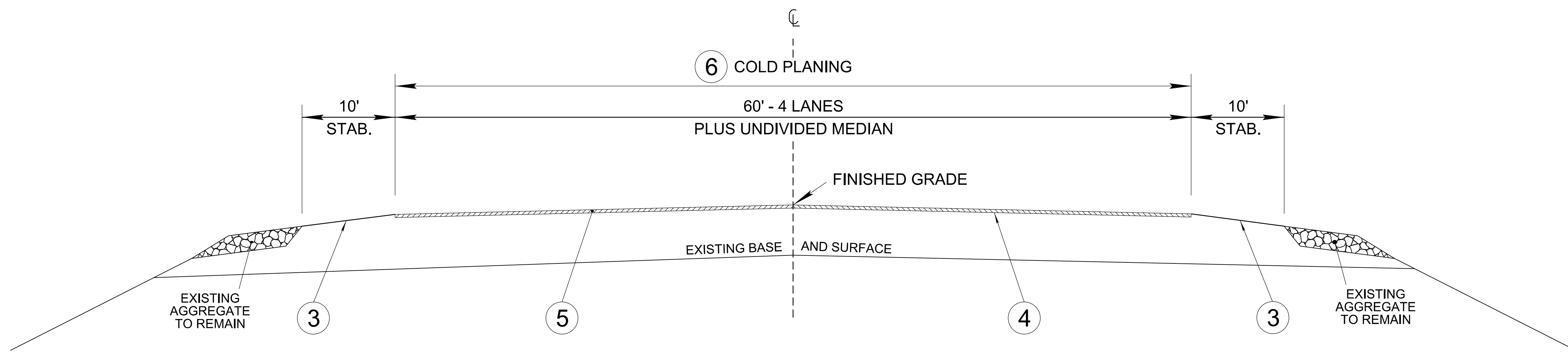
**ESTIMATED
ROADWAY
QUANTITIES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	2B

REV. 06/02/26: REMOVED ALTERNATE AA1 LANGUAGE. REVISED TREATMENT LAYERS AND TREATMENT CALL OUTS. ADDED PAVEMENT SCHEDULE NOTE



FROM: L.M. 14.18 TO L.M. 23.78



FROM: L.M. 23.78 TO L.M. 24.26

SEE SHEET 2B1 FOR PAVEMENT SCHEDULE

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DEPARTMENT OF TRANSPORTATION

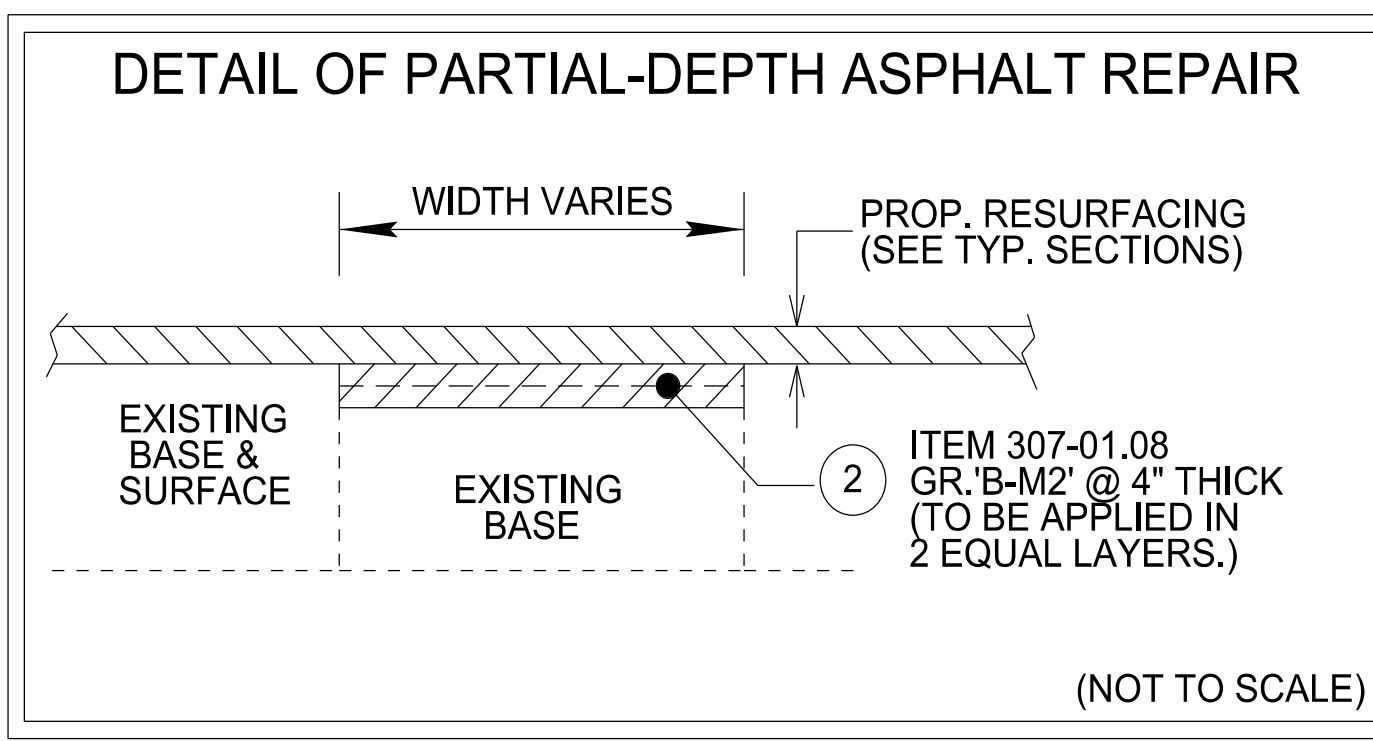
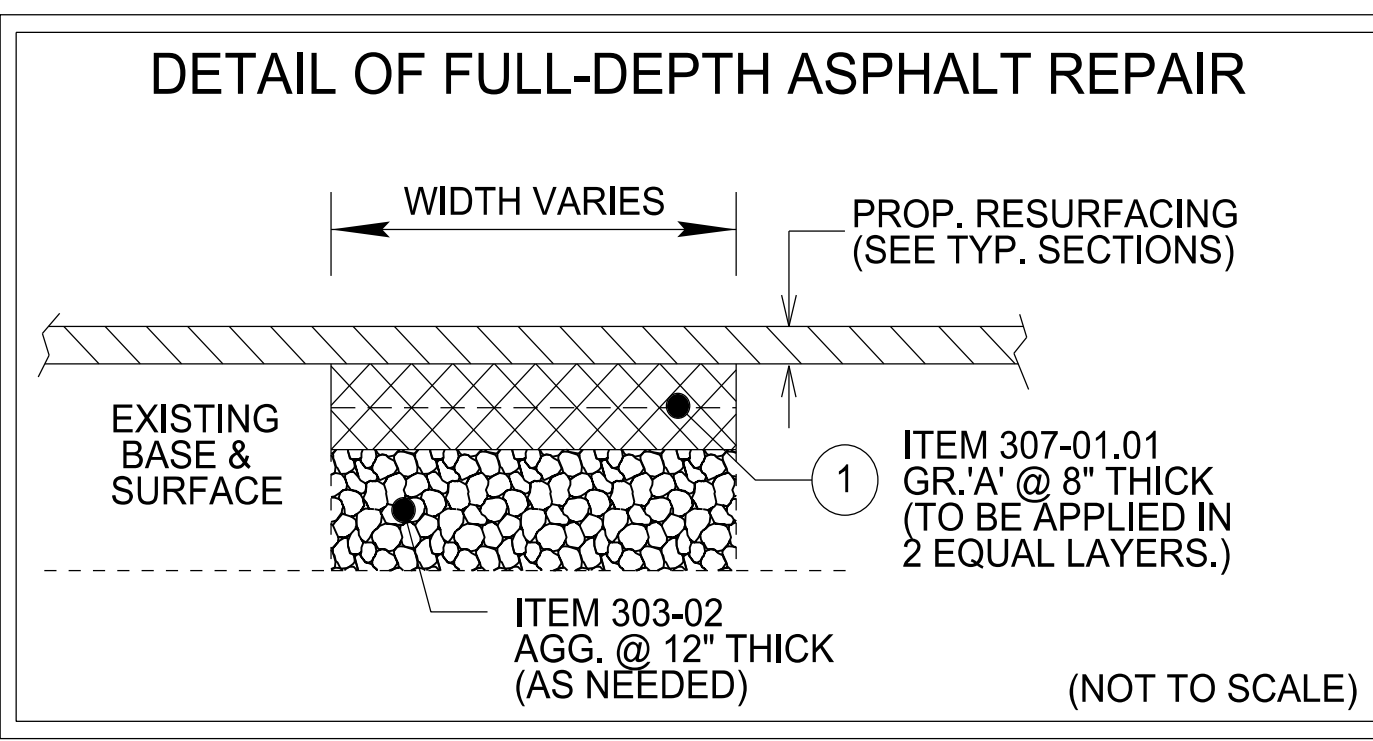
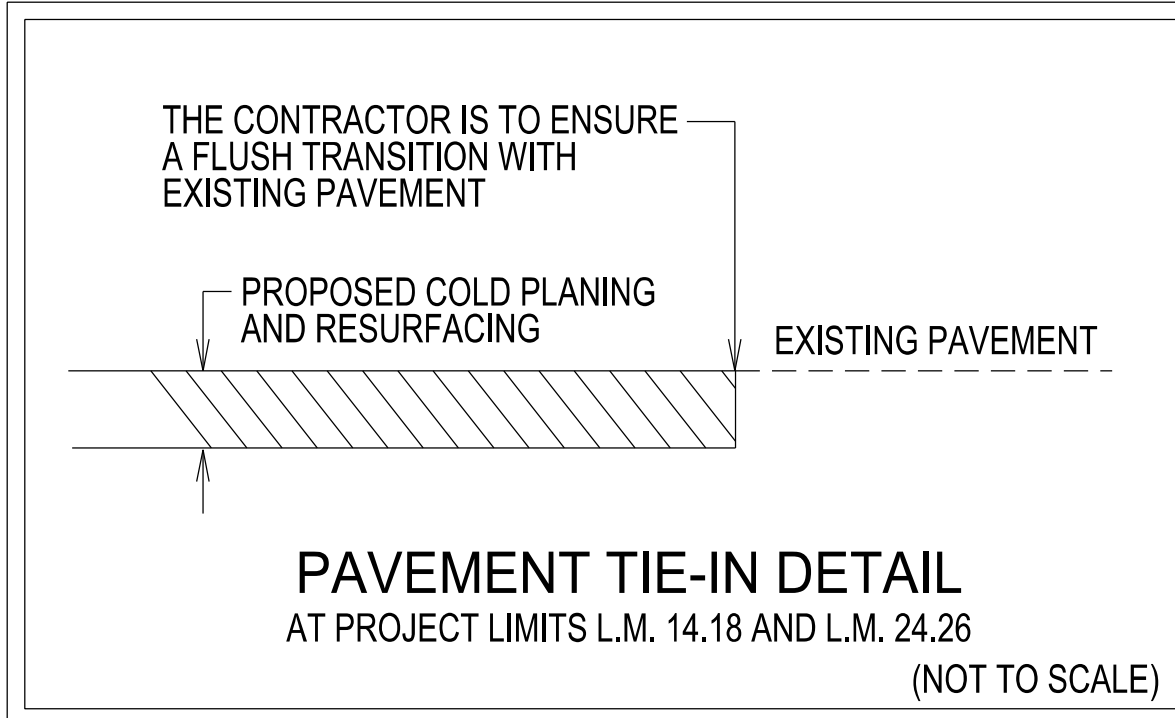
TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	2B1

REV. 06/02/26: REVISED PAVEMENT SCHEDULE TO REFLECT NEW TREATMENT. REMOVED MILLING DETAILS. ADDED BREAKOUT DETAILS AND PAVEMENT TIE-IN DETAIL



PROPOSED PAVEMENT SCHEDULE	
<p>① BITUMINOUS COURSE (BLACK BASE) @ 8.00"± THICK (APPROX. 920.00 LBS./S.Y.) ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A</p>	<p>④ TRACKLESS TACK COAT (TC) ITEM 403-02.01 TRACKLESS TACK COAT SEE 403.01 FOR DETERMINING APPLICATION RATE IN THE FIELD</p>
<p>② BITUMINOUS COURSE (BINDER) @ 2.00"± THICK (APPROX. 226.00 LBS./S.Y.) ITEM 307-01.08 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2</p>	<p>⑤ ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25"± THICK (APPROX. 132.50 LBS./S.Y.) ITEM 411-01.10 ACS MIX (PG64-22) GRADING D</p>
<p>③ FOG SEAL ITEM 403-01.10 HIGH PERFORMANCE FOG SEALS RATE AT 0.10 - 0.15 GAL./S.Y.</p>	<p>⑥ COLD PLANING @ 1.25"± THICK (APPROX. 131.25 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT</p>

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**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE**

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GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

- (1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.
- (3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
 - a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE ENGINEER.
 - b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE ENGINEER.
 - c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.
 - d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

- (5) THE TOP OF THE SIGN FOOTINGS SHALL BE PLACED LEVEL WITH THE GROUND LINE.
- (6) AFTER THE SIGN LOCATIONS HAVE BEEN STAKED, THERE SHALL BE A FIELD INSPECTION AND APPROVAL BY THE APPROPRIATE REGIONAL OPERATIONS OR CONSTRUCTION OFFICE PRIOR TO ORDERING MATERIAL OR INSTALLING THE SUPPORTS. PAYMENT FOR LOCATION AND STAKING SHALL BE INCLUDED IN THE BID PRICE FOR OTHER ITEMS OF CONSTRUCTION. ANY RELOCATION REQUIRED DUE TO INCORRECT INSTALLATION SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.
- (8) THE EXISTING FOOTINGS ARE TO BE REMOVED 6 INCHES BELOW GROUND LINE.
- (9) THE LETTERS, DIGITS, ARROWS, BORDERS, AND ALPHABET ACCESSORIES ON ALL FLAT SHEET SIGNS SHALL BE APPLIED BY THE SILK SCREENING PROCESS, EXCEPT THAT CUT-OUT DIRECT APPLIED COPY SHALL BE USED ON SIGNS WITH GREEN OR BROWN BACKGROUNDS.
- (10) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

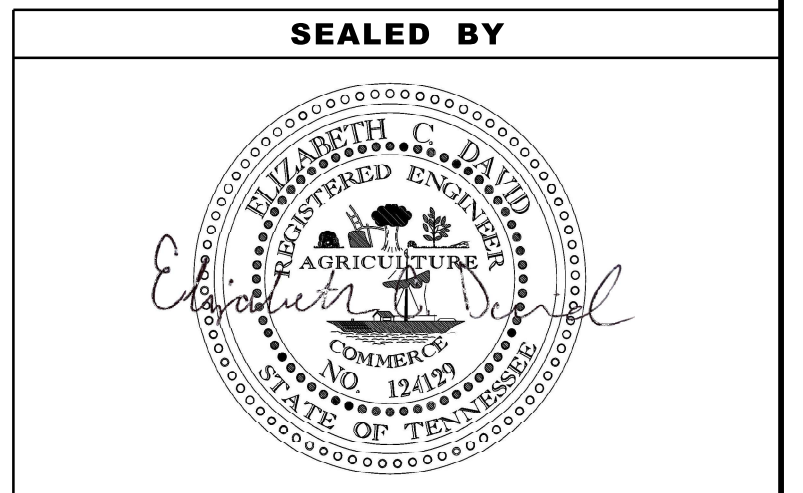
CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	2C

REV. 06/02/26: ADDED NOTES 4 AND 5 IN THE RESURFACING SECTION. REMOVED OUTDATED SIGNING NOTES AND ADDED UPDATED SIGNING NOTES



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES

SPECIAL NOTES

RESURFACING

- (1) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (3) THE INSIDE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE INSIDE TRAFFIC LANE.
- (6) THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 96 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED, EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.
- (8) FEATHER SURFACE MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.

PAVEMENT MARKING

- (1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.
- (2) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

SIGNS

- (1) IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS

- (4) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION. ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS OR NON-WORK DAYS.

RAILROAD NOTES

- (3) THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE WEST TENNESSEE RAILROAD, LLC TRACK FACILITIES AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY WEST TENNESSEE RAILROAD, LLC SO TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAIN TRAFFIC IN ANY MANNER.
- (4) THE CONTRACTOR SHALL NOT, FOR ANY REASON, STORE ANY OF HIS CONSTRUCTION EQUIPMENT OR DUMP WASTE MATERIALS ON THE RAILROAD'S RIGHT-OF-WAY.
- (5) THE CONTRACTOR IS PROHIBITED FROM INSTALLING ANY CONSTRUCTION SIGNS, MESSAGE BOARDS, ARROW BOARDS AND/OR OTHER TEMPORARY TRAFFIC CONTROL SIGNS OR DEVICES WITHIN THE RAILROAD'S RIGHT-OF-WAY. IN ALL CASES, THESE SIGNS AND DEVICES MUST NEVER BE PLACED IN A POSITION SO AS TO OBSTRUCT THE VIEW OF ANY ADVANCE WARNING DEVICES SUCH AS CROSSBUCK SIGNS, CROSSING SIGNALS, ETC.
- (6) THE CONTRACTOR SHALL NOTIFY AND COORDINATE HIS WORK AT THE RAILROAD CROSSING WITH THE FOLLOWING REPRESENTATIVE OF THE RAILROAD:

MR. JJ HOHORST PRESIDENT
 11 EAST CHURCH STREET
 SEA BRIGHT, NJ 07760
 PHONE: (732) 842-0912
 AND
 MR. CHUCK HALLIBURTON - GENERAL MANAGER
 WEST TENNESSEE RAILROAD, LLC
 PHONE: (731) 432-4304
 E-MAIL: CHALLIBURTON@WTNRR.COM

STANDARD SPECIFICATIONS FOR APPLICATION OF CRACK SEALANT

ASPHALT PAVEMENT – CRACK SEALING

I. SCOPE:

- (1) THIS WORK SHALL CONSIST OF FURNISHING ALL LABOR, MATERIAL, AND EQUIPMENT NECESSARY TO PERFORM ALL OPERATIONS FOR THE APPLICATION OF AN IN-DEPTH JOINT SEALING SYSTEM TO PROVIDE CRACK SEALING ON ASPHALTIC CONCRETE PAVEMENT.
- (2) THE CRACK-SEAL MATERIAL SHALL BE APPROVED HOT-POUR MATERIAL LISTED ON THE TDOT QPL (QUALIFIED PRODUCTS LIST) AND THE PRICE SHALL INCLUDE ALL ASPECTS OF INSTALLATION WITHIN REGION 4 WHERE THE CONTRACT IS ACTIVATED.

II. DESCRIPTION:

- (1) THIS WORK SHALL CONSIST OF CLEANING AND FILLING EXISTING LONGITUDINAL AND TRANSVERSE CRACKS HAVING A WIDTH OF 3/16 INCH OR GREATER IN FLEXIBLE PAVEMENTS.

III. METHOD OF APPLICATION:

- (1) ALL CRACKS SHALL BE THOROUGHLY CLEANED WITH HIGH PRESSURE, DRY COMPRESSED AIR REMOVING ALL VEGETATION, DEBRIS, MOISTURE AND FOREIGN MATERIALS, AS DIRECTED BY THE ENGINEER.
- (2) THE SEALANT SHALL BE APPLIED TO THE CRACK WITH A PRESSURE FEED WAND SYSTEM IMMEDIATELY AFTER CLEANING AT A TEMPERATURE WITHIN THE RANGE RECOMMENDED BY THE MANUFACTURER OF THE SEALANT.
- (3) THE SEALANT SHALL BE APPLIED USING THE FLUSH FILL METHOD.
- (4) THE CRACK SHALL BE FILLED LEVEL WITH THE ASPHALT SURFACE. IMMEDIATELY AFTER PLACEMENT OF SEALANT, A V-SHAPED RUBBER SQUEEGEE SHALL BE USED 100% OF THE TIME. THE USE OF SEALING DISCS IS NOT ACCEPTABLE.
- (5) ANY SEALANT ABOVE THE ASPHALT SURFACE MUST BE FEATHERED OUT AS DIRECTED BY THE ENGINEER.
- (6) THE CRACK FILLING WILL ONLY BE ALLOWED WHEN BOTH THE AIR AND PAVEMENT TEMPERATURES ARE WITHIN THE TOLLERANCES RECOMMENDED BY THE MANUFACTURER OF THE MATERIAL.

IV. MATERIALS:

- (1) THE SEALANT SHALL BE AN ASTM D6690 TYPE I PRODUCT LISTED ON THE DEPARTMENT'S QUALIFIED PRODUCT LIST 5 "JOINT SEALERS AND FILLERS", SECTION C- "HOT POUR JOINT SEALERS," MATERIAL "905.05.011."
- (2) STORAGE, HEATING, APPLICATION INSTRUCTIONS AND CAUTIONS SHALL BE SUPPLIED WITH EACH SHIPMENT.
- (3) THE MATERIAL WILL BE KEPT WITHIN THE TEMPERATURE RANGE RECOMMENDED BY THE MANUFACTURER.
- (4) THE CONTRACTOR SHALL FURNISH THE ENGINEER WITH A CERTIFIED STATEMENT FROM THE MANUFACTURER OF THE SEALANT SHOWING COMPLIANCE WITH THIS SPECIFICATION TOGETHER WITH A CERTIFIED COPY OF THE TEST REPORT.

V. EQUIPMENT:

- (1) THE MELTER-APPLICATOR SHALL BE AN OIL JACKETED DOUBLE BOILER TYPE, EQUIPPED WITH AN AGITATOR AND SEPARATE THERMOMETERS FOR BOTH THE OIL BATH AND THE MELTING VAT.
- (2) ALL EQUIPMENT NECESSARY FOR THE SATISFACTORY PERFORMANCE OF THIS OPERATION SHALL BE ON THE JOB AND APPROVED BY THE ENGINEER BEFORE WORK WILL BE PERMITTED TO BEGIN.

VI. METHOD OF MEASUREMENT FOR PAYMENT:

- (1) SEALANT FOR RANDOM CRACKS WILL BE MEASURED BY THE POUND.
- (2) AT THE BEGINNING OF EACH WORK-DAY, THE ENGINEER, OR HIS APPOINTED REPRESENTATIVE, SHALL DOCUMENT THE AMOUNT OF MATERIAL IN THE HEATER-MELTER UNIT AND LOG ALL ADDITIONAL MATERIAL ADDED DURING THE DAY AND MEASURE THE AMOUNT OF MATERIAL REMAINING IN THE HEATER-MELTER AT THE END OF EACH DAY TO DETERMINE THE TOTAL POUNDAGE USED.
- (3) PAYMENT WILL BE MADE BY THE POUND FOR RANDOM CRACKS ANYWHERE BETWEEN THE CENTERLINE AND THE PAVING JOINT BETWEEN THE TRAVEL LANE AND SHOULDER.

VII. TRAFFIC CONTROL:

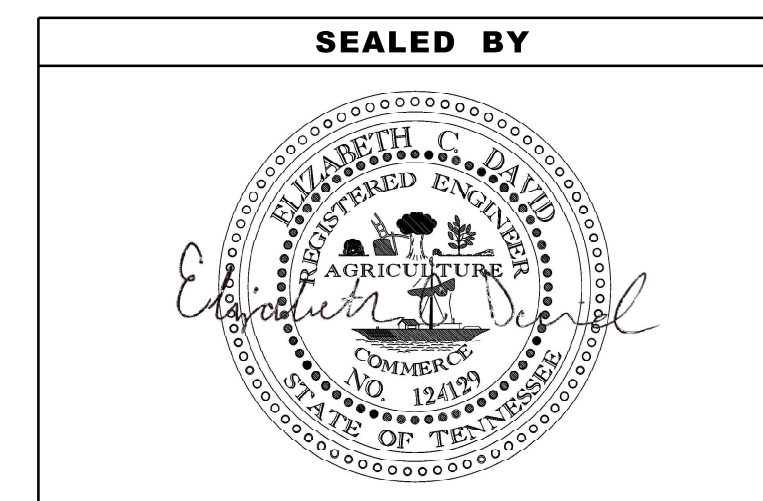
- (1) ALL TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND TDOT STANDARD DRAWINGS.
- (2) ALL TRAFFIC CONTROL DEVICES FOR LANE CLOSURES (CONSTRUCTION SIGNS, PORTABLE ARROW BOARDS, CONES, ETC.) LANE CLOSURES SHALL BE OF SUFFICIENT LENGTH TO ACCOMPLISH SIGNIFICANT WORK.
- (3) WHERE RAISED PAVEMENT MARKERS EXIST, THE CONTRACTOR SHALL PROTECT THE MARKERS DURING THE CLEANING AND SEALING PROCESS.
- (4) THE CONTRACTOR WILL BE REQUIRED TO PLACE TEMPORARY PAINTED PAVEMENT MARKINGS WHERE CRACK FILLING MATERIALS OBLITERATE THE EXISTING MARKINGS. COSTS OF TEMPORARY MARKINGS ARE TO BE INCLUDED IN OTHER ITEMS.

SPECIAL CRACK SEALANT NOTES:

- A. OVER APPLICATION WILL NOT BE ACCEPTED, AND WILL RESULT IN NON-PAYMENT OF SERVICES ON AREAS OF IMPROPER APPLICATION.
- B. THIS CONTRACT IS FOR LONGITUDINAL AND TRANSVERSE CRACKS, JOINTS, AND BLOCK CRACKING ONLY.
- C. THIS CONTRACT IS NOT DESIGNED FOR ALLIGATOR CRACKS THAT APPLY EXCESSIVE AMOUNTS TO THE ROADWAY.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	2D

REV. 06/02/26: ADDED NOTE 3 UNDER THE RESURFACING SECTION



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

SPECIAL
 NOTES

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE PROJECT MANAGER SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL FIELD REVIEW MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRECONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES MILLING THE MAINLINE, INSIDE SHOULDERS, MEDIAN CROSSOVERS & SIDE ROADS AND REPLACING WITH 411D MIX. CRACK AND FOG SEALING THE OUTSIDE SHOULDERS AND ADDRESSING BREAKOUT AREAS. ADDITIONAL WORK INCLUDES UPGRADES TO THE PAVEMENT MARKINGS, THE SNOWPLOWABLE MARKERS AND VARIOUS GUARDRAIL TERMINALS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

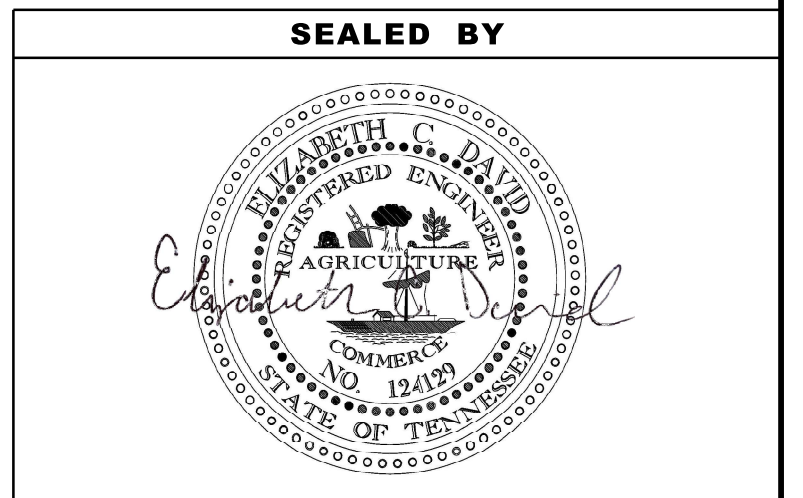
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	2E

REV. 06/02/26: REVISED SCOPE OF WORK NOTE TO INCLUDE NEW TREATMENT TYPE. REVISED ENVIRONMENTAL NOTE ONE



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

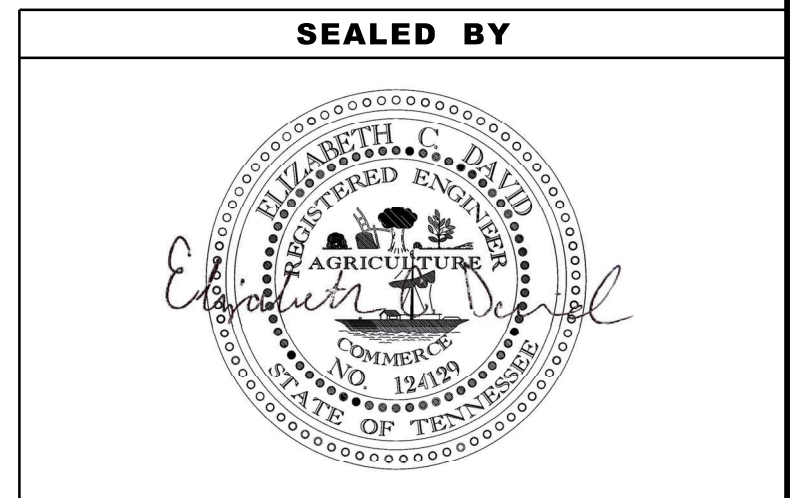
ENVIRONMENTAL
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SHEET 1 OF 2

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ENVIRONMENTAL NOTES CONT.

- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	2E1



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**ENVIRONMENTAL
NOTES**

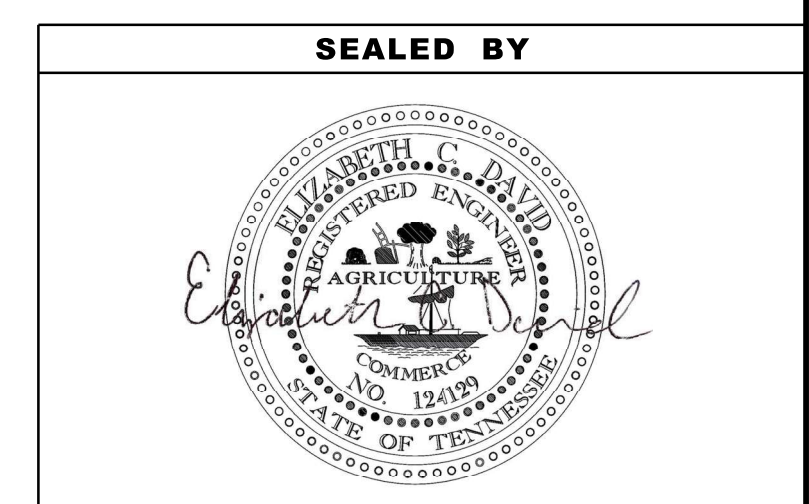
SHEET 2 OF 2

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	2F

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PROPOSED GUARDRAIL (RESURFACING)													
SIDE		LOCATION		LOG MILE	GUARDRAIL				TERMINAL ANCHORS				
					THRIE BEAM BRIDGE TRANS MASH TL-3 (20.65') 705-01.01 (EACH)	SPECIAL HEIGHT TRANSITION 705-02.10 (EACH)	ROUND END ELEMENT 706-10.26 (EACH)	GUARDRAIL REMOVED 706-01 (L.F.)	TYPE 12 (21.875') 705-04.02 (EACH)	TYPE 13 MASH TL3 (9.375') 705-06.10 (EACH)	IN-LINE MASH TL3 705-06.11 (EACH)	TYPE 38 MASH TL3 (26.896') 705-06.20 (EACH)	TYPE 21 MASH TL2 (21.875') 705-06.30 (EACH)
NB	SB	OUT	MED										
X			X	14.989		1		50				1	
X		X		15.024		1		50				1	
X			X	15.170		1		50				1	
X		X		15.418		1		50				1	
X		X		16.791		1		50				1	
X		X		17.605		1		50				1	
X			X	17.618		1		50				1	
X		X		18.083		1		50				1	
X		X		19.230		1		50				1	
X		X		19.689		1		50				1	
X		X		20.440		1		50				1	
X		X		20.950		1		50				1	
X		X		21.074		1		50				1	
X		X		21.422		1		50				1	
X		X		22.251		1		50				1	
X		X		23.633		1		50				1	
X		X		23.972		1		50				1	
	X	X		22.330		1		50				1	
	X	X		21.500		1		50				1	
	X	X		21.173		1		50				1	
	X	X		21.115		1		50				1	
	X	X		20.567		1		50				1	
	X	X		19.442		1		50				1	
	X	X		18.193		1		50				1	
	X		X	17.775		1		50				1	
	X	X		16.822		1		50				1	
	X	X		16.376		1		50				1	
	X	X		15.672		1		50				1	
	X		X	15.325		1		50				1	
	X		X	15.181		1		50				1	
	X	X		14.809		1		50				1	
TOTALS						31		1550				31	

PROPOSED SIGNS AND REMOVAL OF SIGNS								
LOCATION		LOG MILE	REMOVAL OF EXISTING SIGN POST	SIGN TYP. 1 STOP (R1-1)	SIGN TYP. 2 DO NOT ENTER (R5-1)	SIGN TYP. 3 DO NOT ENTER (R5-1) ONE WAY (R6-1L)	SIGN TYP. 4 YIELD (R1-2) NO RIGHT TURN (R3-1)	COMMENTS
OUT	MED		713-15.01 (EACH)	713-16.20 (EACH)	713-16.21 (EACH)	713-16.22 (EACH)	713-16.23 (EACH)	
	X	21.071	1					SIGN: YIELD
	X	21.071		1				SEE STANDARD DRAWING T-S-26 FOR FURTHER DETAILS
	X	21.081			1			SEE STANDARD DRAWING T-S-26 FOR FURTHER DETAILS
	X	21.135				1		SEE STANDARD DRAWING T-S-26 FOR FURTHER DETAILS
	X	21.150	2					SIGN 1: YIELD, DO NOT ENTER & ONE WAY SIGN 2: YIELD & ONE WAY
	X	21.150					2	SEE STANDARD DRAWING T-S-26 FOR FURTHER DETAILS
	X	21.155				1		SEE STANDARD DRAWING T-S-26 FOR FURTHER DETAILS
	X	21.238			1			SEE STANDARD DRAWING T-S-26 FOR FURTHER DETAILS
	X	21.248	1					SIGN: YIELD & DO NOT ENTER
	X	21.248		1				SEE STANDARD DRAWING T-S-26 FOR FURTHER DETAILS
TOTALS			4	2	2	2	2	



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

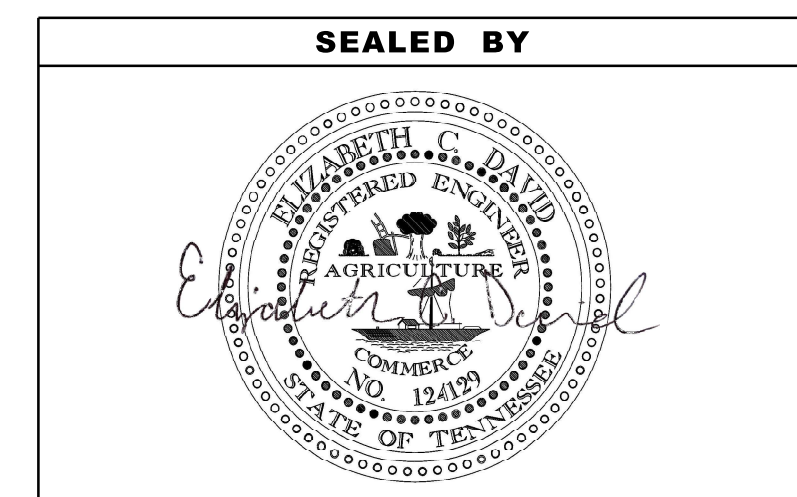
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QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	2F1

REV. 06/02/26: REMOVED ALTERNATIVE LAYOUT FROM SIGN TABULATION BLOCK

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)						
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES		S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
		L	W			
(G20-1)	ROAD WORK NEXT 11 MI.	48"	24"	8	4	32
(G20-2)	END ROAD WORK	48"	24"	8	28	224
(W4-2)	MERGE LEFT	48"	48"	16	4	64
(W4-2)	MERGE RIGHT	48"	48"	16	4	64
(W8-11)	UNEVEN LANES	48"	48"	16	108	1728
(W8-15)	GROOVED PAVEMENT	48"	48"	16	108	1728
(W8-15P)	MOTORCYCLE (PLAQUE)	30"	24"	5	108	540
(W20-1)	ROAD WORK AHEAD	48"	48"	16	24	384
(W20-1)	ROAD WORK 1 MILE	48"	48"	16	4	64
(W20-1)	ROAD WORK 1/2 MILE	48"	48"	16	4	64
(W20-1)	ROAD WORK 1500 FT	48"	48"	16	4	64
(W20-4)	RIGHT LANE CLOSED 1 MILE	48"	48"	16	4	64
(W20-4)	RIGHT LANE CLOSED 1/2 MILE	48"	48"	16	4	64
(W20-5)	RIGHT LANE CLOSED 1500 FT.	48"	48"	16	4	64
(W20-5)	LEFT LANE CLOSED 1 MILE	48"	48"	16	4	64
(W20-5)	LEFT LANE CLOSED 1/2 MILE	48"	48"	16	4	64
(W20-5)	LEFT LANE CLOSED 1500 FT.	48"	48"	16	4	64
(W20-7A)	FLAGGER AHEAD	48"	48"	16	4	64
(W21-2)	FRESH OIL	48"	48"	16	4	64
(W21-5)	SHOULDER WORK	48"	48"	16	44	704
THE CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.					TOTAL	6172

BRIDGE NOTES					
LOCATION		BRIDGE NUMBER	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
L.M. 15.05	RT	27SR0430011	OVERFLOW	430' - 0"	LEAVE AS IS
L.M. 15.05	LT	27SR0430012	OVERFLOW	430' - 0"	LEAVE AS IS
L.M. 15.23	RT	27SR0420013	RUTHERFORD FORK OBION RIVER	182' - 0"	LEAVE AS IS
L.M. 15.23	LT	27SR0430014	RUTHERFORD FORK OBION RIVER	182' - 0"	LEAVE AS IS
L.M. 17.69	RT	27SR0430031	WTNN	278' - 10"	LEAVE AS IS
L.M. 17.69	LT	27SR0430032	WTNN	278' - 10"	LEAVE AS IS
L.M. 18.12	RT	27SR0430033	BEAR CREEK	68' - 0"	PAVE WITH PLANS MIX / TREATMENT TYPE
L.M. 18.12	LT	27SR0430033	BEAR CREEK	68' - 0"	PAVE WITH PLANS MIX / TREATMENT TYPE
L.M. 23.65	RT	27SR0430019	BRANCH	28' - 0"	PAVE WITH PLANS MIX / TREATMENT TYPE
L.M. 23.65	LT	27SR0430019	BRANCH	28' - 0"	PAVE WITH PLANS MIX / TREATMENT TYPE
L.M. 24.02	-	27SR0430021	OVERFLOW	285' - 0"	LEAVE AS IS
L.M. 24.19	-	27SR0430023	OVERFLOW	285' - 0"	LEAVE AS IS



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

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UTILITY

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

CABLE:
CHARTER COMMUNICATIONS
 24 CIRCLE DR
 McKENZIE, TN 38201
 CONTACT: KEITH CHESSER
 OFFICE PHONE: 731 352 1146
 CELL PHONE: 731 621 9552
 EMAIL: KEITH.CHESSER@CHARTER.COM

ELECTRIC:
WEAKLEY COUNTY ELECTRIC
 11181 HWY 22 / P.O. BOX 170
 MARTIN, TN 38237
 CONTACT: HUNTER WOOD
 OFFICE PHONE: 731 587 9521
 EMAIL: HWOOD@WCMES.COM

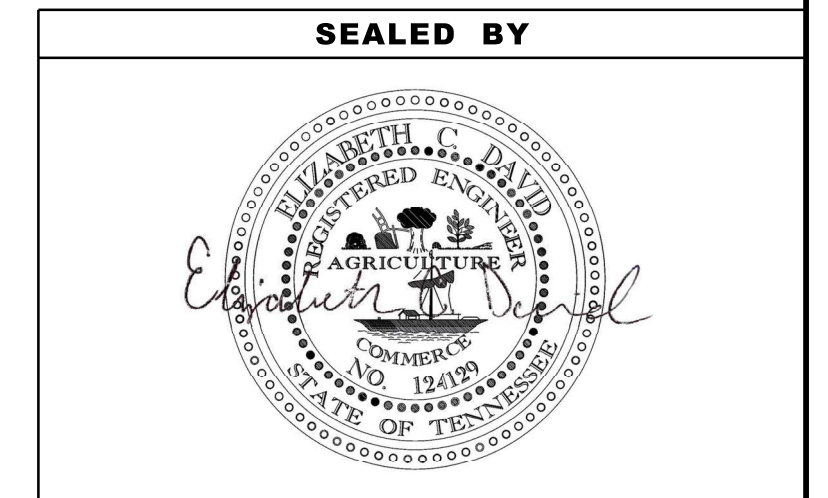
TELEPHONE:
WEST TN TELEPHONE COMPANY (TEC)
 224 EAST MAIN ST
 BRADFORD, TN 38316
 CONTACT: JOEL BRANSON
 OFFICE PHONE: 731 487 6880
 EMAIL: JBRANSON@TEC.COM

COMMUNICATIONS:
AT&T
 315 E. COLLEGE ST
 JACKSON, TN 38301
 CONTACT: DANIEL R. POTTS
 OFFICE PHONE: 901 488 2359
 EMAIL: DP7607@ATT.COM

GAS:
GIBSON COUNTY UTILITY DISTRICT
 1300 HWY 45 BYPASS N. / P.O. BOX 350
 TRENTON, TN 38382
 CONTACT: ALLYSON HORNER
 OFFICE PHONE: 731 855 1441
 EMAIL: ALLYSON@GCUD.NET

WATER:
GIBSON CO.
 153 MILAN HWY
 TRENTON, TN 38382
 CONTACT: TOMMY CASTLEMAN
 OFFICE PHONE: 731 855 0411
 EMAIL: TOMMYCASTLEMAN58@GMAIL.COM

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	3



**STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION**

**UTILITY NOTES
 AND
 UTILITY OWNERS**

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PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2026	NH/HSIP-43(50)	T1

REV. 06/02/26: REVISED NOTE A.1.a. TO THE LATEST GUIDELINES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
 - a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
 - b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:
 - a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO 6 INCHES OR LESS.
- d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES. SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

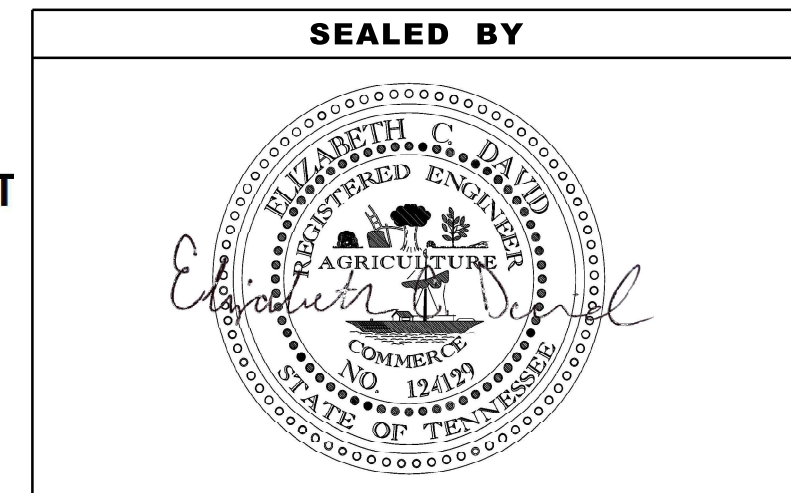
1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.
 - a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
 - b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

- SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL